

Path: I:\NYS-OGS\2068\73070\WO-1-REDI-PLANNING\DWG\SHEETS\PORT BAY FILENAME: 73070.300 - G-001.DWG PLOT DATE: 12/24/2019 1:38 PM CAD USER: ALISON WETHERBEE

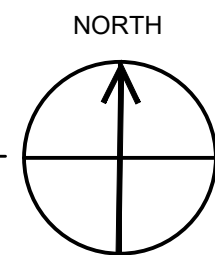
46224 REDI DREDGING PROGRAM PHASE I

PORT BAY OUTLET CHANNEL DREDGING

PORT BAY HURON, NEW YORK, 14590



SITE LOCATION MAP
NOT TO SCALE



CONTRACT NO. 1

INDEX TO DRAWINGS

- TITLE SHEET
- G-1 GENERAL NOTES
- G-2 SITE PLAN
- G-3 DREDGING
- G-4 CROSS SECTIONS
- G-5 CROSS SECTIONS
- G-6 MISCELLANEOUS DETAILS

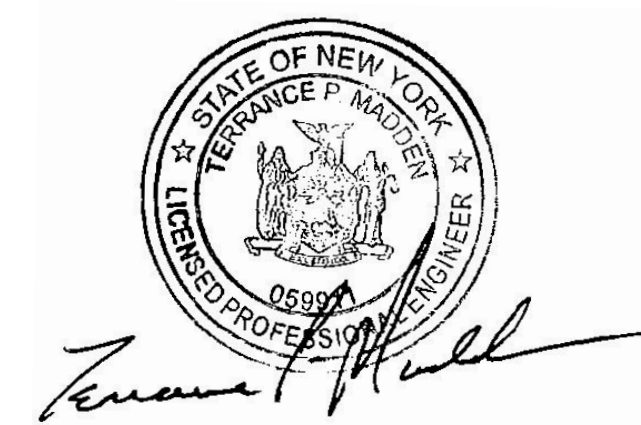
OFFICE OF GENERAL SERVICES
NEW YORK STATE

DECEMBER 2019



DESIGN & CONSTRUCTION

O'BRIEN & GERE ENGINEERS, INC
A RAMBOLL COMPANY



IT IS A VIOLATION OF LAW FOR ANY PERSON UNLESS ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER TO ALTER THIS DOCUMENT.

Path: I:\NYS-OGS-2068\73070.WC-1-REDIPLANNING\DWG\SHEETS\PORT BAY - G-1.DWG PLOT DATE: 12/24/2019 2:08 PM CAD USER: ALLISON WETHERBEE

THE SCOPE OF WORK IS GENERALLY DESCRIBED AS FOLLOWS:

- REMOVE SEDIMENTS WITHIN THE PORT BAY OUTLET CHANNEL TO THE HORIZONTAL AND VERTICLE EXTENTS SHOWN.
- ALL WORK SHALL BE PERFORMED FROM THE WESTERN SHORELINE.
- ACTUAL EXTENTS OF DREDGING WILL BE BASED ON THE REACH OF APPROVED EQUIPMENT.
- REQUIRED DREDGE DEPTH = 237.3 INTERNATIONAL GREAT LAKES DATUM 1985
- MAXIMUM ALLOWABLE OVER-DREDGE = 0.5- FEET
- STOCKPILE DREDGED SEDIMENTS WITHIN THE SPOIL AREA IDENTIFIED ON SHEET G-3.
- FOR PURPOSES OF DEVELOPING SUBCONTRACTORS FEE A VOLUME OF 1,350 CUBIC YARD SHOULD BE ASSUMED. ACTUAL VOLUME REMOVED WILL BE DEPENDENT ON THE CONDITIONS AT THE TIME OF CONSTRUCTION AND SUBCONTRACTORS MEANS AND METHODS.
- WORK TO BE PERFORMED IN ACCORDANCE WITH THE PROJECT PERMITS
 - NYSDEC PERMIT NOS. 8-5426-00010/00007, 00008, AND /00009
 - USACE LETTER OF PERMISSION DATED SEPTEMBER 13, 2018.

GENERAL NOTES - ALL DRAWINGS:

1. EXISTING SITE INFORMATION PRESENTED SHOULD BE CONSIDERED APPROXIMATE ONLY.
2. EXACT DIMENSIONS AND LOCATIONS OF ALL STRUCTURES AND UTILITIES ARE CONSIDERED APPROXIMATE ONLY AND SHALL BE VERIFIED AS REQUIRED IN THE FIELD BY THE SUBCONTRACTOR.
3. CONTRACTOR IS RESPONSIBLE FOR MAINTAINING ACCESS ROADWAY DURING CONSTRUCTIONS AND REPAIR OF ANY DAMAGE CAUSED BY CONTRACTORS ACTIVITIES.
4. UNDERGROUND UTILITIES MAY EXIST. THE LOCATIONS, DEPTHS AND EXTENT OF WHICH ARE UNKNOWN. THE SUBCONTRACTOR SHALL DETERMINE THE LOCATION AND ELEVATION OF ALL UTILITIES IN THE FIELD AS IT MAY PERTAIN TO THE SUBCONTRACTORS WORK PRIOR TO CONSTRUCTION.
5. DURING CONSTRUCTION THE SUBCONTRACTOR IS RESPONSIBLE FOR THE PROTECTION AND SUPPORT OF ALL UNDER AND ABOVE GROUND UTILITIES AFFECTED BY THE SUBCONTRACTOR'S WORK.
6. THE SUBCONTRACTOR SHALL CONTACT "DIG SAFELY NY" WITHIN 72 HOURS PRIOR TO THE COMMENCEMENT OF THE WORK. THE SUBCONTRACTOR SHALL VERIFY THE LOCATION OF ALL UTILITIES AND IF NECESSARY, NOTIFY THE AFFECTED UTILITY DEPARTMENTS ONE WEEK PRIOR TO DIGGING IN ANY PORTION OF THE SITE. DIG SAFELY NEW YORK PHONE NUMBER: 1-800-962-7962. WEBSITE: WWW.DIGSAFELYNEWYORK.COM
7. THE SUBCONTRACTOR SHALL COORDINATE ANY NECESSARY TRAFFIC CONTROLS AND OBTAIN ANY NECESSARY PERMITS THAT MAY BE REQUIRED TO PERFORM THE WORK.
8. THE SUBCONTRACTOR SHALL BE RESPONSIBLE FOR ESTABLISHING AND MAINTAINING SURVEY CONTROL DURING THE PERFORMANCE OF WORK AND TO VERIFY EXISTING GRADES. THE CONTRACT DRAWINGS WILL BE PROVIDED TO THE SUBCONTRACTOR IN ELECTRONIC FORMAT FOR THE SUBCONTRACTOR'S USE.
9. PROPER EROSION CONTROL TECHNIQUES SHALL BE IMPLEMENTED AS REQUIRED IN ACCORDANCE WITH THE APPLICABLE REGULATIONS OR DIRECTED BY THE ENGINEER.
10. THE SUBCONTRACTOR SHALL RESTORE TO PRECONSTRUCTION CONDITIONS OR BETTER ALL SUPPORT AREAS THAT ARE IMPACTED BY CONSTRUCTION ACTIVITIES.
11. ALL SURFACES DAMAGED OR DESTROYED AS A RESULT OF WORK PERFORMED BY THE SUBCONTRACTOR SHALL BE RESTORED TO PRE-CONSTRUCTION CONDITIONS OR BETTER IN A TIMELY MANNER AND PRIOR TO SUBCONTRACTOR DEMOBILIZATION.
12. THE SUBCONTRACTOR MUST COMPLY WITH PERMIT CONDITIONS AND MAINTAIN A COPY OF ALL PERMITS ON SITE DURING CONSTRUCTION.
13. THE SUBCONTRACTOR SHALL NOT INVADE UPON PRIVATE PROPERTIES, LANDS OR BUILDING FOR ANY REASON WITHOUT FIRST SECURING PERMISSION FOR THE PROPERTY OWNER. THE CONTRACTOR SHALL BE HELD LIABLE FOR ANY DAMAGE THAT MAY OCCUR. ENGINEER ASSUMES NO RESPONSIBILITY FOR DAMAGE OR INJURY DUE TO SUBCONTRACTOR USE OF PRIVATE PROPERTY.

SURVEY NOTES - ALL DRAWINGS

1. THIS SURVEY IS REFERENCED HORIZONTALLY TO THE NORTH AMERICAN DATUM OF 1983 (NAD83), PROJECTED ON THE NEW YORK STATE PLANE COORDINATE SYSTEM (CENTRAL ZONE).
2. THIS SURVEY IS REFERENCED VERTICALLY TO THE INTERNATIONAL GREAT LAKES DATUM OF 1985 (IGLD85).
3. NORTH ARROW AS SHOWN INDICATES GRID NORTH REFERENCED TO NAD83 AND PROJECTED ON THE NEW YORK STATE PLANE COORDINATE SYSTEM (CENTRAL ZONE).
4. THE INFORMATION SHOWN HEREON IS BASED ON A BATHYMETRIC AND FIELD SURVEY COMPLETED ON DECEMBER 4, 2019 BY PRUDENT ENGINEERING OF EAST SYRACUSE, NEW YORK.
5. THE CONTRACTOR SHALL HAVE A LICENSED SURVEYOR PERFORM A SINGLE-BEAM BATHYMETRIC SURVEY OF THE ENTIRE CHANNEL AREA WITHIN 72-HOURS OF THE START OF DREDGING. SURVEY SHALL BE PROVIDED TO THE ENGINEER BY REQUEST AND SHALL BE CONSIDERED THE PRE-CONSTRUCTION SURVEY FOR PURPOSES OF MEASUREMENT FOR PAYMENT.
6. ALL COORDINATION OF FIELD SURVEY OPERATIONS IS THE RESPONSIBILITY OF THE CONTRACTOR.
7. THE SURVEY SHALL HAVE A MINIMUM HORIZONTAL ACCURACY OF 1-FT AND A MINIMUM VERTICAL ACCURACY OF 0.1-FT

DREDGING AND STOCKPILING REQUIREMENTS:

1. DREDGING OPERATIONS SHALL BE CONDUCTED TO THE ENGINEER'S SATISFACTION, AND IN ACCORDANCE WITH REGULATORY PERMITS, THE CONTRACT DRAWINGS AND THE SUBCONTRACTOR SCOPE OF WORK.
2. MINIMIZE SPILLAGE, RETURN, AND RE-SUSPENSION OF DREDGED MATERIALS TO THE WATERS OF NEW YORK.
3. QUANTITIES RESULTING FROM DREDGING GREATER THAN 6 INCHES BELOW THE REQUIRED DREDGE LINE WILL NOT BE PAID FOR.
4. ANY CONTIGUOUS 400 SF AREA WHOSE AVERAGE ELEVATION IS GREATER THAN THE REQUIRED DREDGE LINE SHALL BE RE -DREDGED IF DIRECTED BY THE ENGINEER.
5. THE TARGED DREDGING PRISM INCLUDES THE DESIRED DEPTH WITHIN THE DREDGING LIMITS SHOWN ON THE CONTRACT DRAWINGS.
6. ONLY MINIMAL DISRUPTION TO PUBLIC USE OF THE OUTLET CHANNEL FOR TRAVEL BETWEEN PORT BAY AND LAKE ONTARIO WILL BE ALLOWED.
7. DREDGED SEDIMENT SHALL BE STOCKPILED TO A HEIGHT NO GREATER THAN 10-FT FROM SURROUNDING GRADE. THE COST FOR SUCH WORK SHALL BE INCLUDED IN THE DREDGING PAYMENT ITEM.
8. AT THE COMPLETION OF WORK THE STOCKPILE SHALL BE COVERED WITH 40 MIL POLYETHYLENE SHEETING AND PROPERLY SECURED AND SILT FENCING SHALL BE PLACED TO COMPLETELY ENCIRCLE THE STOCKPILE. THE COST FOR SUCH WORK SHALL BE INCLUDED IN DREDGING PAYMENT ITEM.
9. CONTRACTOR SHALL TAKE MEASURES TO PREVENT EROSION BACK TO SURFACE WATERS. SILT FENCE, OR OTHER APPROPRIATE AND APPROVED MEANS, SHALL BE PLACED AS NEEDED AND/OR DIRECTED BY THE ENGINEER.
10. CONTRACTOR SHALL REMOVE AND REINSTALL OR PROTECT EXISTING RAILING AT BULKHEAD.
11. CONTRACTOR SHALL UTILIZE CRANE MATS OR APPROVED EQUAL TO MITIGATE EQUIPMENT LOADS ON EXISTING BULKHEAD. DAMAGE TO THE BULKHEAD SHALL BE REPAIRED BY THE CONTRACTOR AT NO ADDITIONAL COST.

ENVIRONMENTAL PROCEDURES

1. THE CONTRACTOR SHALL CONDUCT OPERATIONS, TO THE SATISFACTION OF THE ENGINEER, TO PREVENT OR REDUCE TO A MINIMUM ANY DAMAGE TO ANY WATERWAY FROM POLLUTION BY DEBRIS, SEDIMENT, OR OTHER FOREIGN MATERIAL, OR FROM MANIPULATION OF EQUIPMENT AND/OR MATERIALS IN OR NEAR THE WATERWAY. THE CONTRACTOR SHALL NOT RETURN DIRECTLY TO A STREAM, OR TO A DITCH IMMEDIATELY FLOWING INTO A STREAM, ANY WATER WHICH HAS BEEN USED FOR WASH PURPOSES OR OTHER SIMILAR OPERATIONS, WHICH COULD CAUSE THIS WATER TO BECOME POLLUTED WITH SAND, SILT, CEMENT, OIL, OR OTHER IMPURITIES. IF THE CONTRACTOR USES WATER FROM A STREAM, THE CONTRACTOR SHALL CONSTRUCT AN INTAKE OR TEMPORARY DAM TO PROTECT AND MAINTAIN WATER RIGHTS, AND TO SUSTAIN FISH LIFE DOWNSTREAM. THE CONTRACTOR'S OPERATIONS SHALL BE CONDUCTED IN A MANNER THAT MAINTAINS FLOW CONSISTENT WITH AMBIENT CONDITIONS.
2. NO VISIBLE CONTRAST BETWEEN UPSTREAM AND DOWNSTREAM WATER QUALITY SHALL BE ALLOWED. ALL TEMPORARY MEASURES SHALL BE REMOVED, AND THE AREA RESTORED, TO THE SATISFACTION OF THE ENGINEER, AT THE COMPLETION OF THE WORK.
3. APPROPRIATE SPILL PREVENTION AND CONTROL PROCEDURES SHALL BE IMPLEMENTED PRIOR TO ANY CONSTRUCTION ACTIVITIES, TO PREVENT OIL AND OTHER SUCH MATERIALS FROM DISCHARGING TO THE GROUND, DRAINS, DITCHES, SURFACE WATERS, WETLANDS AND/OR GROUNDWATER. THESE PROCEDURES SHALL INCLUDE, BUT ARE NOT LIMITED TO, PROPER MAINTENANCE OF CONSTRUCTION EQUIPMENT, DESIGNATION OF FUEL/HAZARDOUS SUBSTANCES HANDLING AREAS, USE OF APPROPRIATE CONTAINMENT AND SPILL RESPONSE RESOURCES, FOR PREVENTION OF ANY SPILLS FROM ENTERING THE ENVIRONMENT.
4. AN EMERGENCY RESPONSE CONTAINMENT KIT, INCLUDING OIL ABSORBENT BOOMS AND PADS, SHALL BE RETAINED ON SITE FOR RAPID DEPLOYMENT TO SOAK UP ANY POSSIBLE SPILLAGE.
5. THE USE OF CHEMICAL DISPERSING AGENTS AND EMULSIFIERS IS NOT AUTHORIZED WITHOUT PRIOR SPECIFIC FEDERAL OR STATE APPROVAL.
6. ALL FUELS WILL BE STORED IN A DOT-APPROVED METAL SAFETY CONTAINER WITH SELF-CLOSING SPOUT COVERS AND FLAME ARRESTORS.
7. ANY QUANTITY OF MATERIAL RELEASED TO SURFACE WATER SHALL REQUIRE A SPILL RESPONSE
8. ALL SPILL RESPONSE PROCEDURES SHALL BE IN ACCORDANCE WITH UNITED STATES COAST GUARD AND NEW YORK STATE DEPARTMENT OF ENVIRONMENTAL CONSERVATION (NYSDEC) REGULATIONS.
9. IN THE EVENT OF A RELEASE GREATER THAN 5 GALLONS, THE FOLLOWING NOTIFICATION SHALL BE MADE:
 - 9.1. PROJECT ENGINEER.
 - 9.2. NYSDEC - 1-800-457-7362, AND
 - 9.3. AND THE NATIONAL RESPONSE CENTER - 1-800-424-8802
10. IN THE EVENT OF A RELEASE OF LESS THAN 5 GALLONS, THE PROJECT ENGINEER SHALL BE NOTIFIED

WARNING:
THE ALTERATION OF THIS MATERIAL IN ANY WAY, UNLESS DONE UNDER THE DIRECTION OF A COMPARABLE PROFESSIONAL, I.E. ARCHITECT FOR AN ARCHITECT, ENGINEER FOR AN ENGINEER OR LANDSCAPE ARCHITECT FOR A LANDSCAPE ARCHITECT, IS A VIOLATION OF THE NEW YORK STATE EDUCATION LAW AND/OR REGULATIONS AND IS A CLASS 'A' MISDEMEANOR.



O'BRIEN & GERE ENGINEERS, INC
A RAMBOLL COMPANY



**2019
PORT BAY
DREDGING
IMPROVEMENTS**

REVISIONS		
REV	DATE	DESCRIPTION
0	12/24/19	ISSUED FOR CONSTRUCTION

LINE IS 2 INCHES AT FULL SIZE

DESIGNED: R. NEWBY
DRAWN: A. WETHERBEE
CHECKED: T. STANIEC
CHECKED: S. ANAGNOST
APPROVED: T. MADDEN

FILENAME
73070.300 - G-1.DWG
PROJECT NUMBER
73707
CLIENT PROJECT NUMBER
46224

GENERAL NOTES

DRAWING NUMBER
G-1
SHEET NUMBER
2 OF 7

Path: I:\NYS-OGS-2069\73070\VO-1-REDI-PLAN\DWG\SHEETS\PORT BAY FILENAME: 73070.300 - G-2.DWG PLOT DATE: 12/24/2019 2:23 PM CAD USER: ALLISON WETHERBEE



LEGEND

- 245 --- BATHMETRIC MAJOR CONTOUR
- 255 --- BATHMETRIC MINOR CONTOUR
- UPLAND MAJOR CONTOUR
- UPLAND MINOR CONTOUR
- ~~~~~ WOODED AREA
- - - - - APPROXIMATE ALLOWABLE WORK AREA
- - - - - OF - - - - - OF - - - - - TEMPORARY CONSTRUCTION FENCING
- - - - - PERMITTED PORT BAY OUTLET CHANNEL
- - - - - TARGETED DREDGE PRISM
- CHANNEL MARKER

WARNING:
 THE ALTERATION OF THIS MATERIAL IN ANY WAY, UNLESS DONE UNDER THE DIRECTION OF A COMPARABLE PROFESSIONAL, I.E. ARCHITECT FOR AN ARCHITECT, ENGINEER FOR AN ENGINEER OR LANDSCAPE ARCHITECT FOR A LANDSCAPE ARCHITECT, IS A VIOLATION OF THE NEW YORK STATE EDUCATION LAW AND/OR REGULATIONS AND IS A CLASS 'A' MISDEMEANOR.



O'BRIEN & GERE ENGINEERS, INC
 A RAMBOLL COMPANY



2019
 PORT BAY
 DREDGING
 IMPROVEMENTS

REVISIONS		
REV	DATE	DESCRIPTION
0	12/24/19	ISSUED FOR CONSTRUCTION

LINE IS 2 INCHES AT FULL SIZE

DESIGNED: R. NEWBY
 DRAWN: A. WETHERBEE
 CHECKED: T. STANIEC
 CHECKED: S. ANAGNOST
 APPROVED: T. MADDEN
 FILENAME: 73070.300 - G-2.DWG
 PROJECT NUMBER: 73070
 CLIENT PROJECT NUMBER: 46224

SITE PLAN

DRAWING NUMBER
G-2
 SHEET NUMBER
 3 OF 7



Path: I:\NYS-OGS-2069\73070\WO-1-REDI-PLANNING\DWGS\DWGS\PORT BAY - G-3.DWG FILENAME: 73070.300 - G-3.DWG PLOT DATE: 12/24/2019 2:50 PM CAD USER: ALLISON WETHERBEE



LEGEND

- 245 --- BATHMETRIC MAJOR CONTOUR
- 255 --- BATHMETRIC MINOR CONTOUR
- 255 --- UPLAND MAJOR CONTOUR
- 255 --- UPLAND MINOR CONTOUR
- ~~~~~ WOODED AREA
- - - - - APPROXIMATE ALLOWABLE WORK AREA
- - - - - TEMPORARY CONSTRUCTION FENCING
- 240 — PROPOSED MAJOR CONTOUR
- 240 — PROPOSED MINOR CONTOUR
- SF — SF — SF — PROVIDE SILT FENCE
- CHANNEL MARKER

NOTES:

1. REMOVE AND REINSTALL OR PROTECT EXISTING RAILING AT BULKHEAD.
2. PROTECT EXISTING TREES. NO CLEARING SHALL BE ALLOWABLE.
3. UTILIZE CRANE MATTING OR SIMILAR TO MITIGATE EQUIPMENT LOADS ON EXISTING BULKHEAD. DAMAGE TO THE BULKHEAD SHALL BE REPAIRED BY THE CONTRACTOR AT NO ADDITIONAL COST.
4. THE CONTRACTOR SHALL PROVIDE A 3H:1V SLOPE AT THE NORTHERN DREDGING EXTENTS TO MATCH EXISTING GRADES TO THE NORTH. TOP OF BANK IS ANTICIPATED TO OCCUR AT APPROXIMATELY STA 1+29 AND BOTTOM OF BANK IS ANTICIPATED TO OCCUR AT APPROXIMATELY STA 1+46.
5. DREDGING SHALL NOT OCCUR WITHIN 10-FT OF THE BULKHEAD.
6. MEASUREMENT AND PAYMENT SHALL BE AS DESCRIBED IN THE SPECIAL PROVISIONS.

WARNING:
THE ALTERATION OF THIS MATERIAL IN ANY WAY, UNLESS DONE UNDER THE DIRECTION OF A COMPARABLE PROFESSIONAL, I.E. ARCHITECT FOR AN ARCHITECT, ENGINEER FOR AN ENGINEER OR LANDSCAPE ARCHITECT FOR A LANDSCAPE ARCHITECT, IS A VIOLATION OF THE NEW YORK STATE EDUCATION LAW AND/OR REGULATIONS AND IS A CLASS 'A' MISDEMEANOR.



O'BRIEN & GERE ENGINEERS, INC
A RAMBOLL COMPANY



2019
PORT BAY
DREDGING
IMPROVEMENTS

REVISIONS

REV	DATE	DESCRIPTION
0	12/24/19	ISSUED FOR CONSTRUCTION

LINE IS 2 INCHES AT FULL SIZE

DESIGNED: R. NEWBY
DRAWN: A. WETHERBEE
CHECKED: T. STANIEC
CHECKED: S. ANAGNOST
APPROVED: T. MADDEN
FILENAME: 73070.300 - G-3.DWG
PROJECT NUMBER: 73070
CLIENT PROJECT NUMBER: 46224

DREDGING AND STOCKPILE PLAN

DRAWING NUMBER

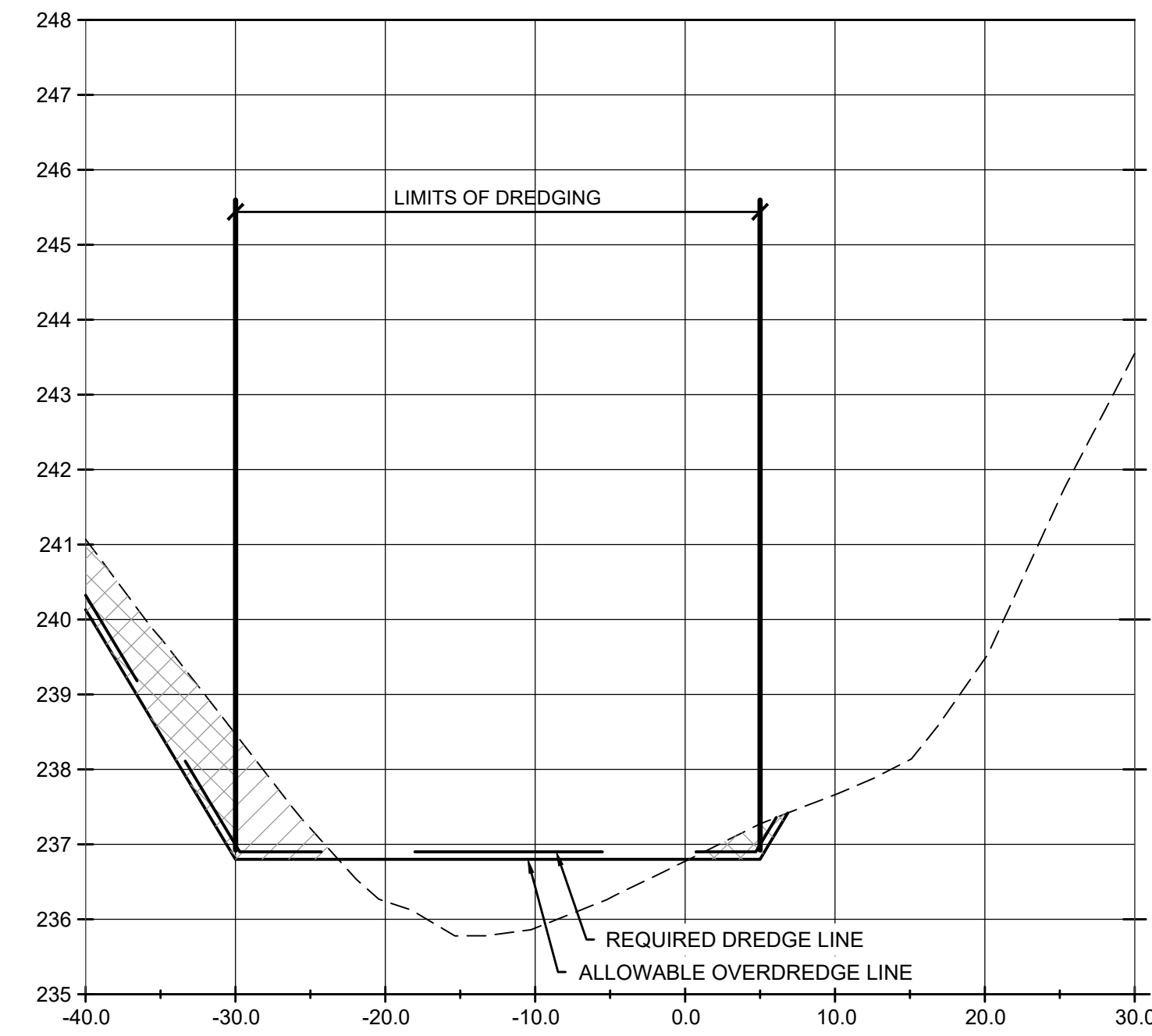
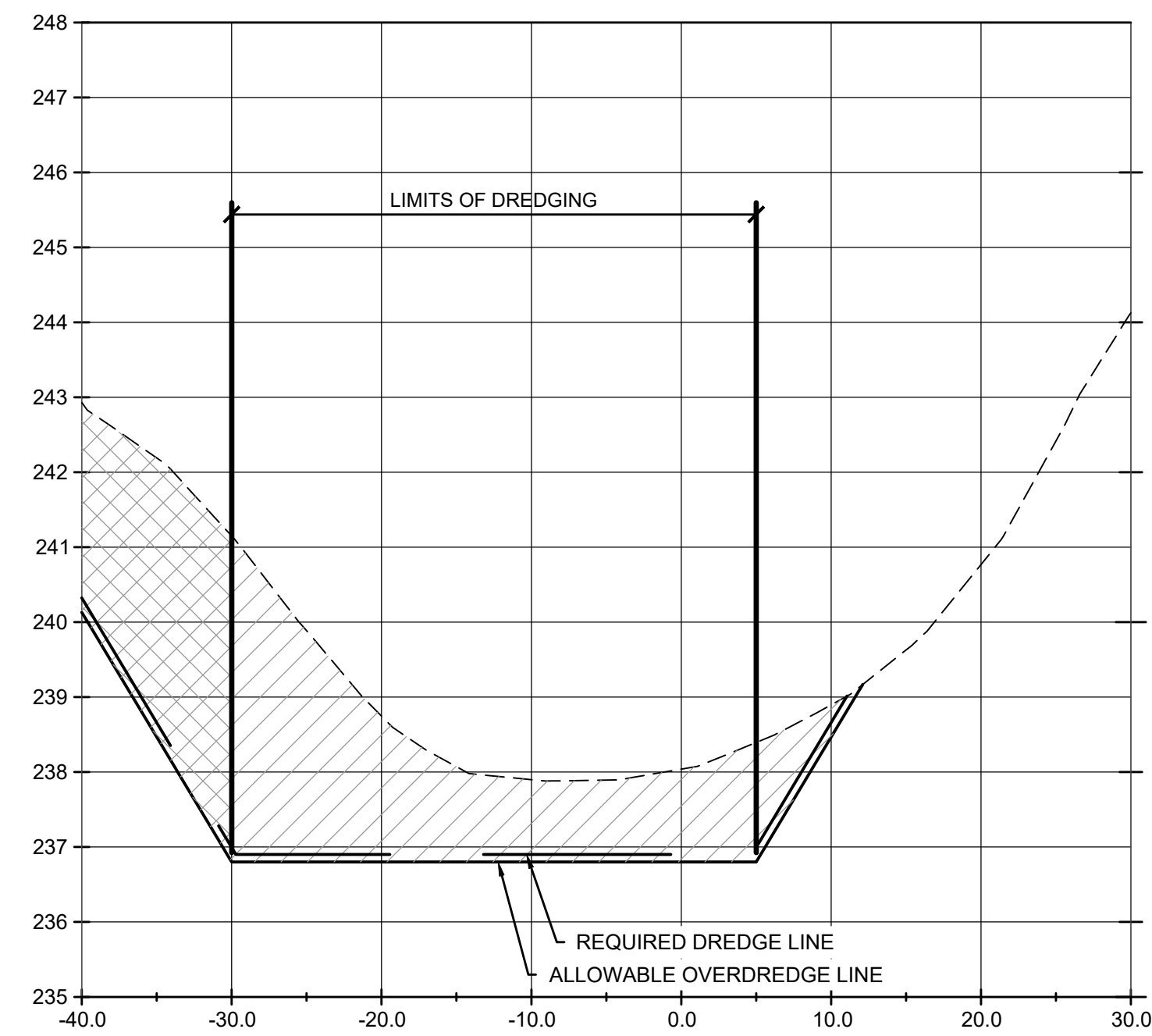
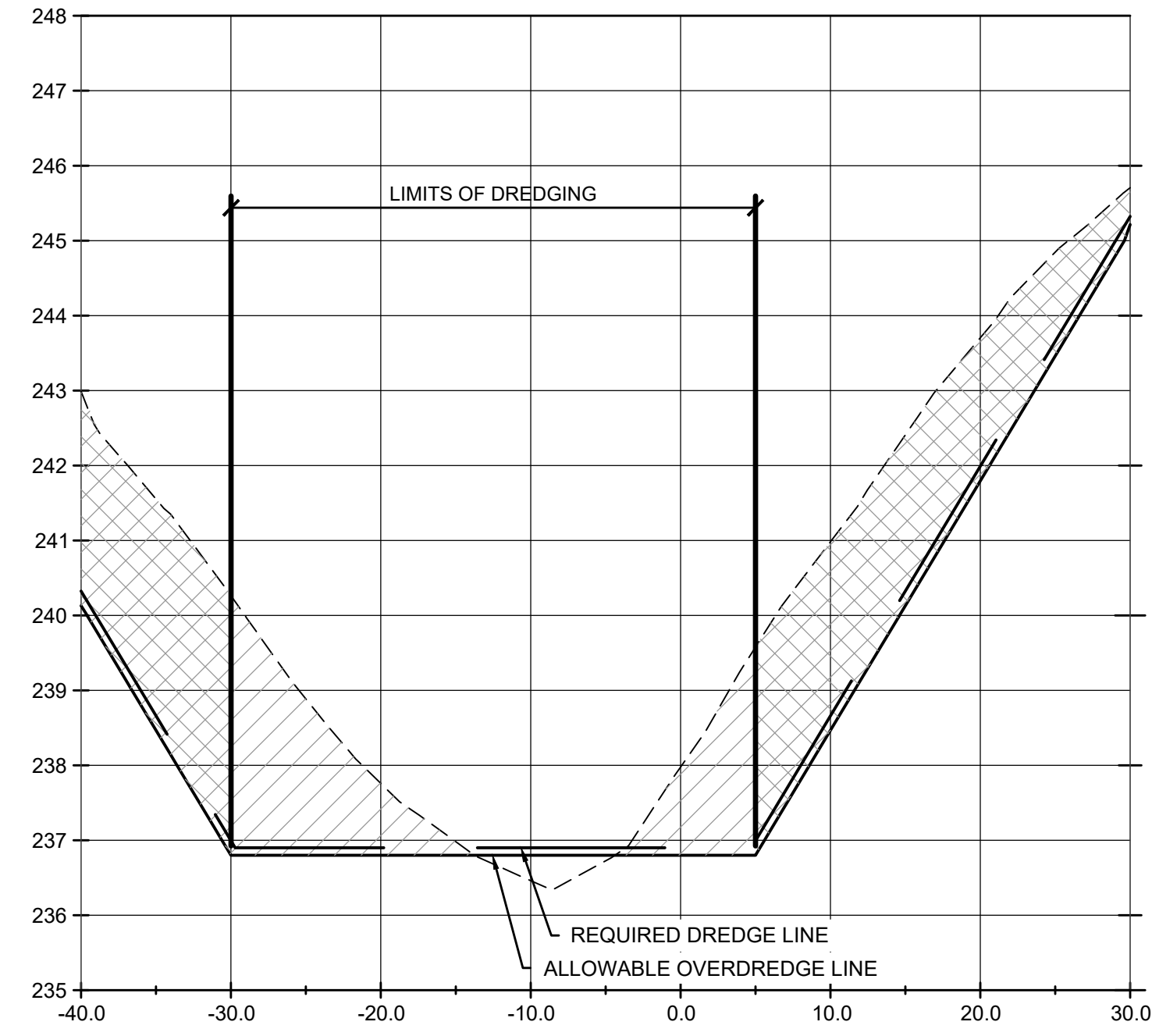
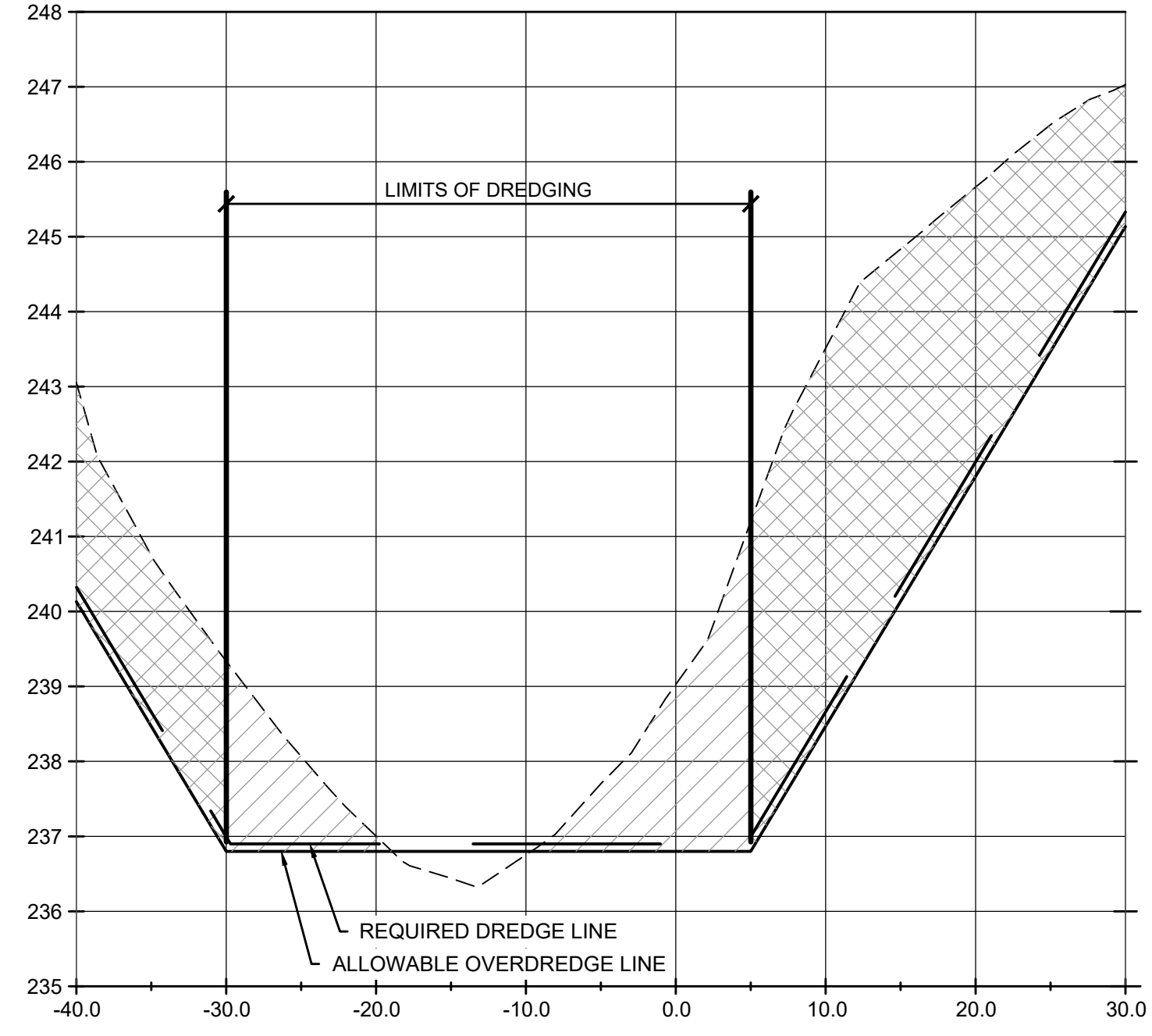
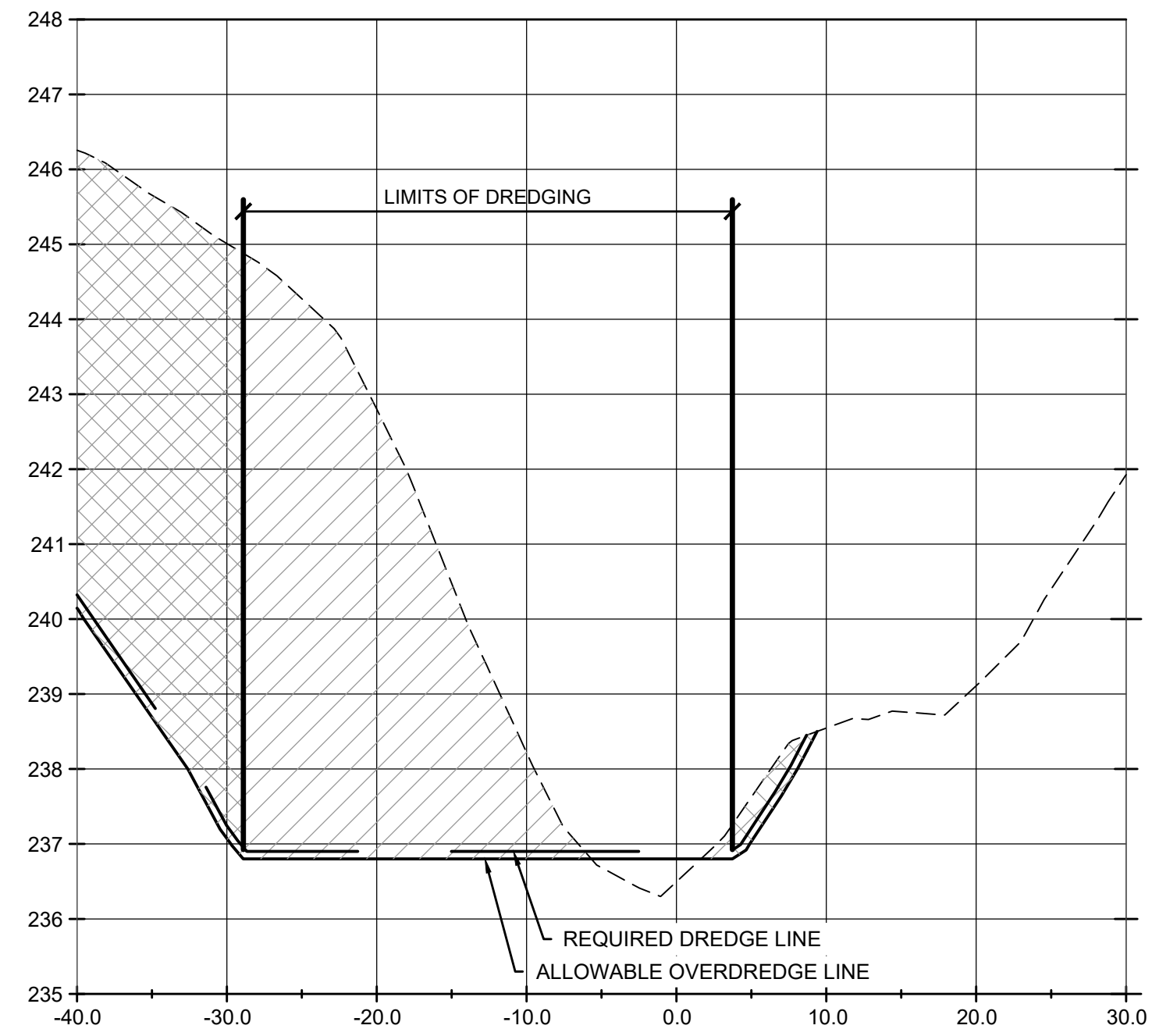
G-3

SHEET NUMBER
4 OF 7

PLAN VIEW



Path: I:\NYS-OGS-2069\73070.WC-1-REDI\PLANNING\DOCS\SHEETS\PORT BAY FILENAME: 73070.300 - G-4 CROSS SECTIONS.DWG PLOT DATE: 12/24/2019 3:03 PM CAD USER: ALLISON WETHERBEE



LEGEND

- EXISTING SEDIMENT ELEVATION
- _____ ALLOWABLE OVERDREDGE LINE
- _____ REQUIRED DREDGE LINE
- ▨ ANTICIPATED ADJACENT AREA SLOUGHING
- ▨ NEATLINE VOLUME

NOTES:

1. HORIZONTAL DISTANCE LABELED AS "0" SHALL BE CONSIDERED THE PORT BAY CHANNEL CENTERLINE. THE ESTABLISHED PORT BAY CHANNEL EXTENDS 30-FEET TO EITHER SIDE OF THE CENTERLINE.
2. THE PERMITTED HORIZONTAL LIMITS OF DREDGING ASSUMES VERTICAL SIDEWALLS AT THE LIMITS OF DREDGING. NO DREDGING SHALL OCCUR OUTSIDE THE SHOWN LIMITS.
3. SLOPES EXTENDING OUTSIDE THE SHOWN LIMITS OF DREDGING REPRESENT ANTICIPATED ADJACENT MATERIAL SLOUGHING. MATERIAL THAT ENTERS THE CUT TEMPLATE DUE TO SLOUGHING DURING THE COURSE OF DREDGING FROM OUTSIDE THE LIMITS SHALL BE REMOVED AS TARGET SEDIMENT.
4. CUT DEPTHS SHOWN ON THE DRAWINGS ARE BASED ON THE REFERENCED BATHYMETRIC SURVEY ONLY. ACTUAL CUT DEPTHS WILL BE BASED ON SEDIMENT ELEVATIONS AT THE TIME OF WORK. THE REMOVAL ELEVATIONS AND LIMITS REPRESENTED BY THE DESIGNED EXTENTS ARE NOT SUBJECT TO CHANGE AND WILL NOT VARY BASED ON CHANGES IN SITE BATHYMETRY.
5. CUT DEPTHS INDICATED ON THIS SHEET ARE BASED ON AN ASSUMED REACH OF TYPICAL EQUIPMENT (SEE DETAIL SHEET G-4). ACTUAL EASTWARD LIMITS OF DREDGING WILL BE BASED ON THE ACHIEVABLE REACH OF THE CONTRACTOR'S APPROVED EQUIPMENT WHICH MAY BE GREATER OR LESSOR THAN INDICATED.
6. SEDIMENT REMAINING ABOVE THE MINIMUM DREDGE DEPTH SHALL BE REMOVED AS DIRECTED.
7. PAYMENT WILL BE BASED ON THE ACTUAL CUBIC YARDS OF SEDIMENT REMOVED WITHIN THE DREDGE TEMPLATE AS MEASURED BY THE DIFFERENCE BETWEEN AN APPROVED PRE-DREDGE SURVEY AND POST DREDGE SURVEYS.
8. NO PAYMENT SHALL BE MADE FOR SEDIMENT REMOVED BELOW THE ALLOWABLE OVERDREDGE LINE.

WARNING:
THE ALTERATION OF THIS MATERIAL IN ANY WAY, UNLESS DONE UNDER THE DIRECTION OF A COMPARABLE PROFESSIONAL, I.E. ARCHITECT FOR AN ARCHITECT, ENGINEER FOR AN ENGINEER OR LANDSCAPE ARCHITECT FOR A LANDSCAPE ARCHITECT, IS A VIOLATION OF THE NEW YORK STATE EDUCATION LAW AND/OR REGULATIONS AND IS A CLASS 'A' MISDEMEANOR.



O'BRIEN & GERE ENGINEERS, INC
A RAMBOLL COMPANY



**2019
PORT BAY
DREDGING
IMPROVEMENTS**

REVISIONS

REV	DATE	DESCRIPTION
0	12/24/19	ISSUED FOR CONSTRUCTION

LINE IS 2 INCHES
AT FULL SIZE

DESIGNED: R. NEWBY

DRAWN: A. WETHERBEE

CHECKED: T. STANIEC

CHECKED: S. ANAGNOST

APPROVED: T. MADDEN

FILENAME
73070.300 - G-4 CROSS SECTIONS.DWG
PROJECT NUMBER
73070
CLIENT PROJECT NUMBER
46224

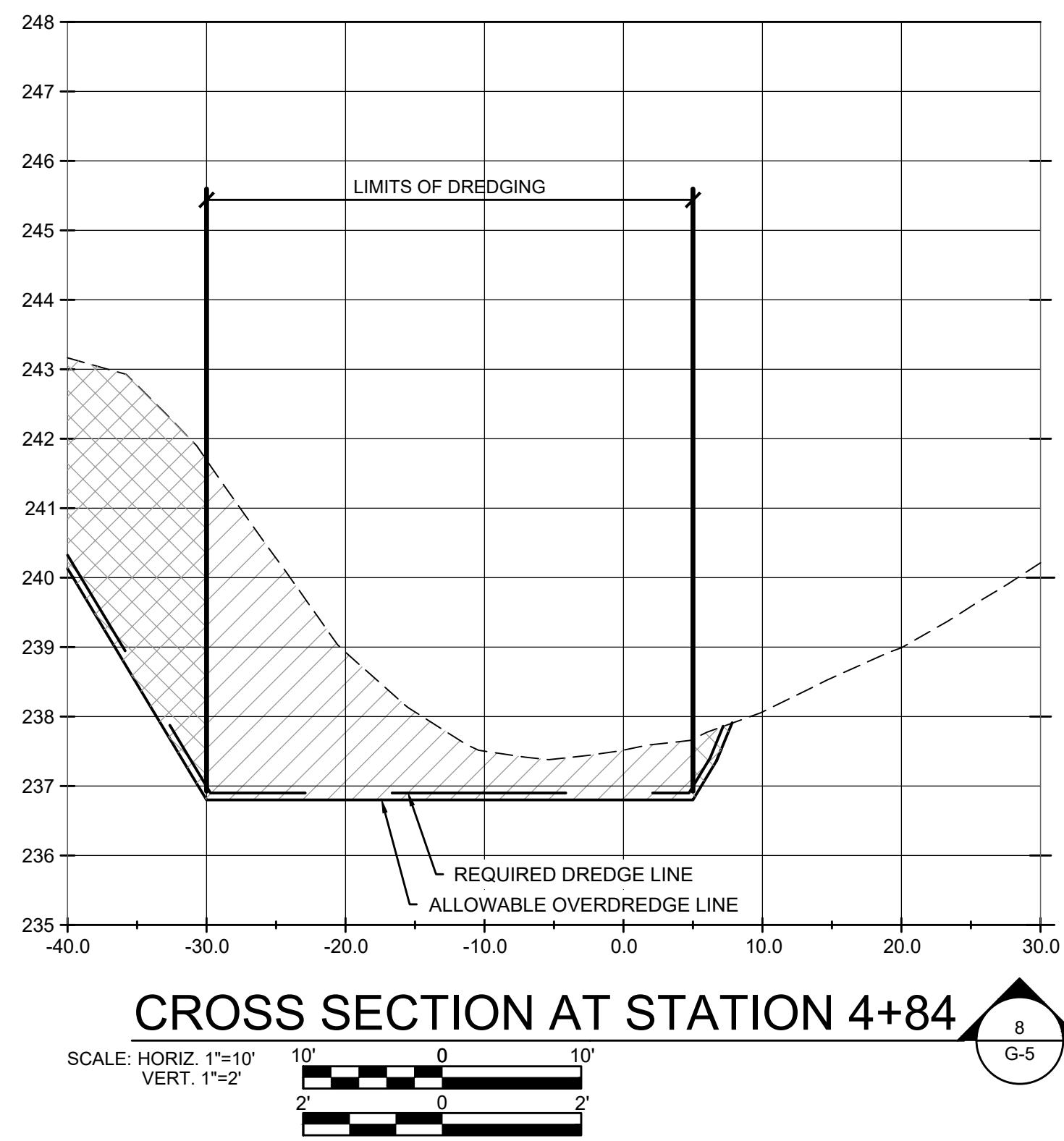
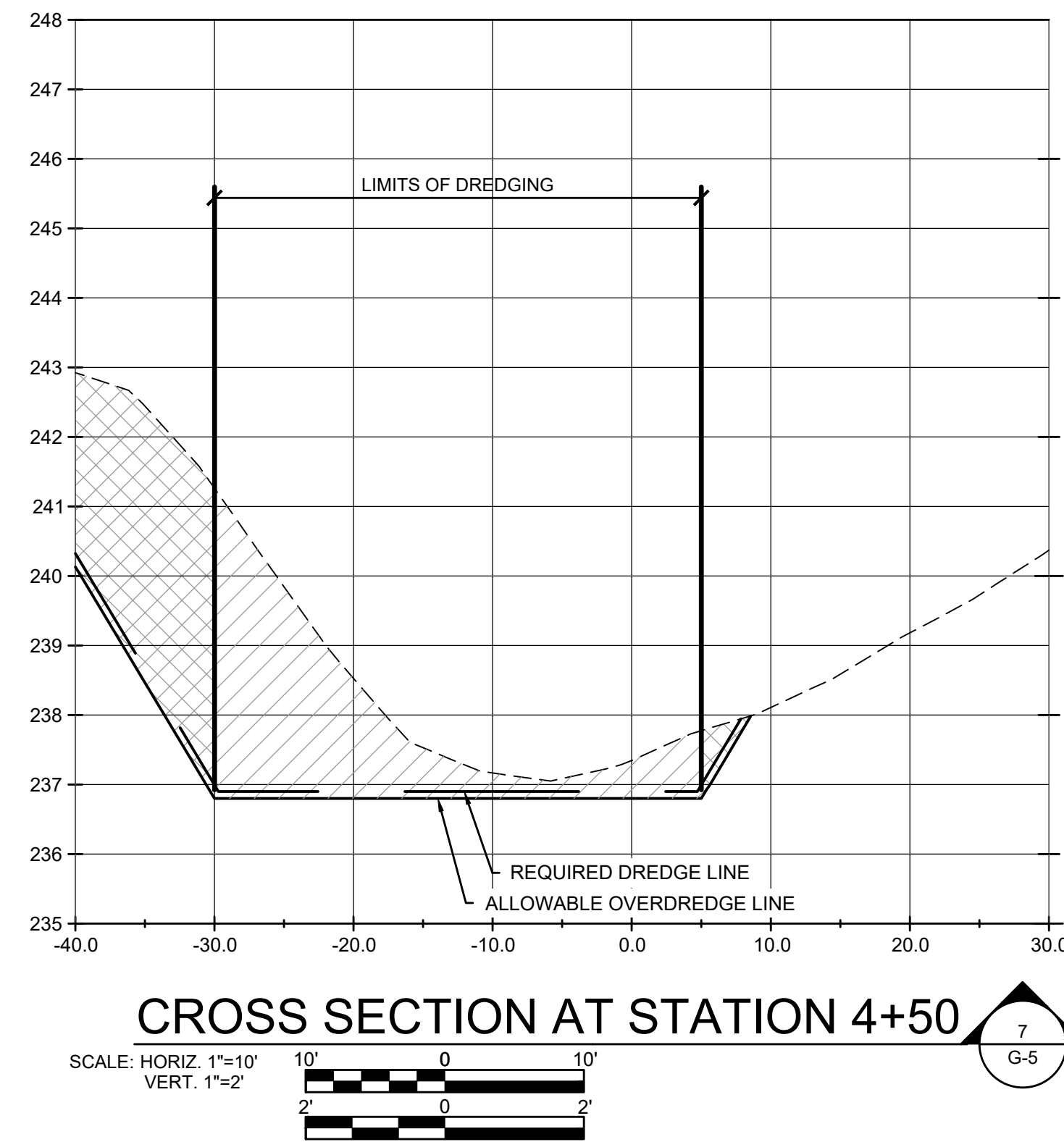
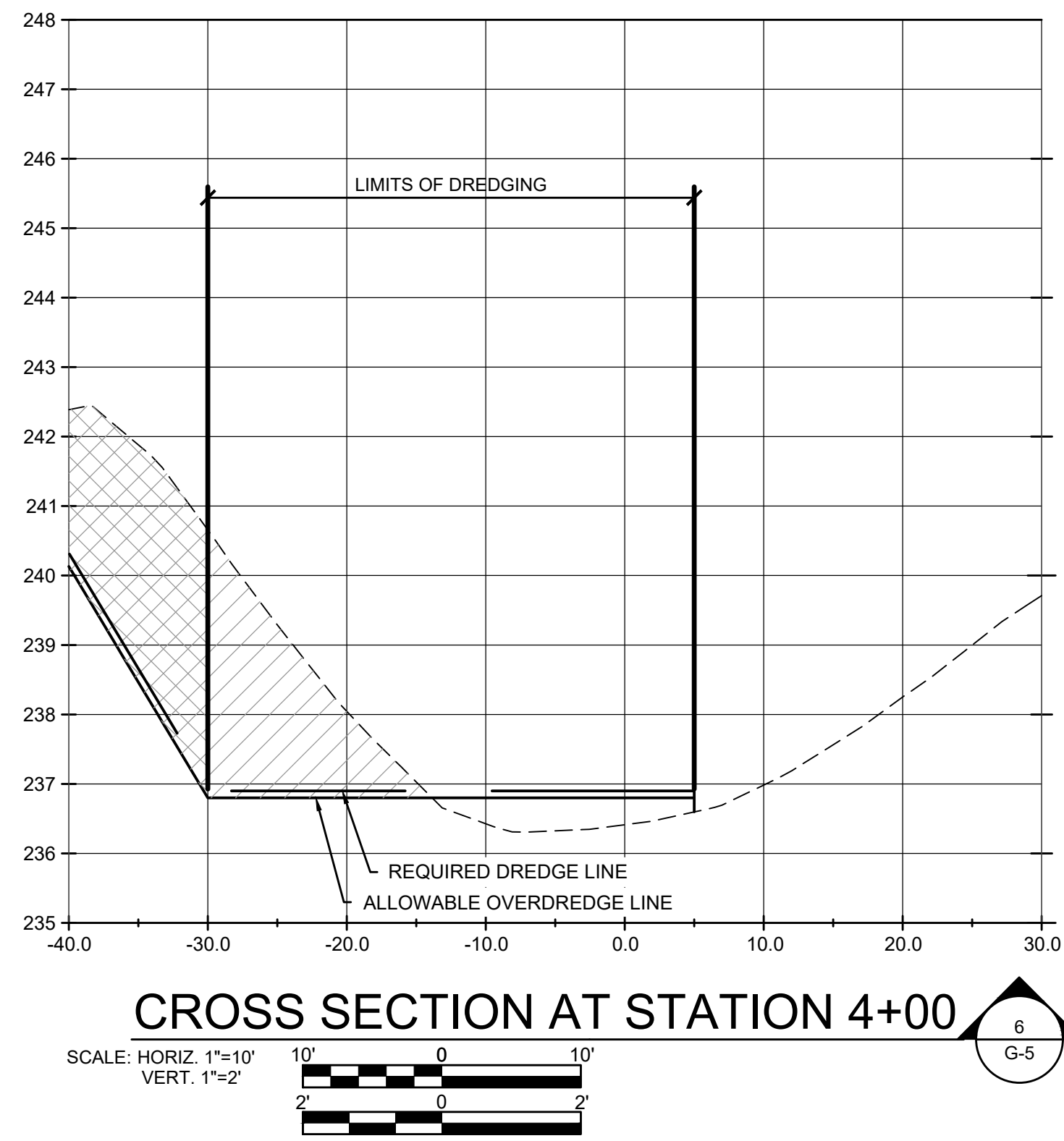
CROSS SECTIONS

DRAWING NUMBER

G-4

SHEET NUMBER
5 OF 7

Path: I:\NYS-OGS-2068\73070.WC-1-REDI-PLANNING\DWG\SHEETS\PORT BAY FILENAME: 73070.300 - G-5 CROSS SECTIONS.DWG PLOT DATE: 12/24/2019 3:10 PM CAD USER: ALLISON WETHERBEE



LEGEND

- EXISTING SEDIMENT ELEVATION
- ALLOWABLE OVERDREDGE LINE
- REQUIRED DREDGE LINE
- [Hatched Area] ANTICIPATED ADJACENT AREA SLOUGHING
- [Hatched Area] NEATLINE VOLUME

NOTES:

1. HORIZONTAL DISTANCE LABELED AS "0" SHALL BE CONSIDERED THE PORT BAY CHANNEL CENTERLINE. THE ESTABLISHED PORT BAY CHANNEL EXTENDS 30-FEET TO EITHER SIDE OF THE CENTERLINE.
2. THE PERMITTED HORIZONTAL LIMITS OF DREDGING ASSUMES VERTICAL SIDEWALLS AT THE LIMITS OF DREDGING. NO DREDGING SHALL OCCUR OUTSIDE THE SHOWN LIMITS.
3. SLOPES EXTENDING OUTSIDE THE SHOWN LIMITS OF DREDGING REPRESENT ANTICIPATED ADJACENT MATERIAL SLOUGHING. MATERIAL THAT ENTERS THE CUT TEMPLATE DUE TO SLOUGHING DURING THE COURSE OF DREDGING FROM OUTSIDE THE LIMITS SHALL BE REMOVED AS TARGET SEDIMENT.
4. CUT DEPTHS SHOWN ON THE DRAWINGS ARE BASED ON THE REFERENCED BATHYMETRIC SURVEY ONLY. ACTUAL CUT DEPTHS WILL BE BASED ON SEDIMENT ELEVATIONS AT THE TIME OF WORK. THE REMOVAL ELEVATIONS AND LIMITS REPRESENTED BY THE DESIGNED EXTENTS ARE NOT SUBJECT TO CHANGE AND WILL NOT VARY BASED ON CHANGES IN SITE BATHYMETRY.
5. CUT DEPTHS INDICATED ON THIS SHEET ARE BASED ON AN ASSUMED REACH OF TYPICAL EQUIPMENT (SEE DETAIL SHEET G-4). ACTUAL EASTWARD LIMITS OF DREDGING WILL BE BASED ON THE ACHIEVABLE REACH OF THE CONTRACTOR'S APPROVED EQUIPMENT WHICH MAY BE GREATER OR LESSOR THAN INDICATED.
6. SEDIMENT REMAINING ABOVE THE MINIMUM DREDGE DEPTH SHALL BE REMOVED AS DIRECTED.
7. PAYMENT WILL BE BASED ON THE ACTUAL CUBIC YARDS OF SEDIMENT REMOVED WITHIN THE DREDGE TEMPLATE AS MEASURED BY THE DIFFERENCE BETWEEN AN APPROVED PRE-DREDGE SURVEY AND POST DREDGE SURVEYS.
8. NO PAYMENT SHALL BE MADE FOR SEDIMENT REMOVED BELOW THE ALLOWABLE OVERDREDGE LINE.

WARNING:
THE ALTERATION OF THIS MATERIAL IN ANY WAY, UNLESS DONE UNDER THE DIRECTION OF A COMPARABLE PROFESSIONAL, I.E. ARCHITECT FOR AN ARCHITECT, ENGINEER FOR AN ENGINEER OR LANDSCAPE ARCHITECT FOR A LANDSCAPE ARCHITECT, IS A VIOLATION OF THE NEW YORK STATE EDUCATION LAW AND/OR REGULATIONS AND IS A CLASS 'A' MISDEMEANOR.



O'BRIEN & GERE ENGINEERS, INC
A RAMBOLL COMPANY



**2019
PORT BAY
DREDGING
IMPROVEMENTS**

REVISIONS

REV	DATE	DESCRIPTION
0	12/24/19	ISSUED FOR CONSTRUCTION

LINE IS 2 INCHES
AT FULL SIZE

DESIGNED: R. NEWBY
DRAWN: A. WETHERBEE
CHECKED: T. STANIC
CHECKED: S. ANAGNOST
APPROVED: T. MADDEN
FILENAME
73070.300 - G-5 CROSS SECTIONS.DWG
PROJECT NUMBER
73070
CLIENT PROJECT NUMBER
46224

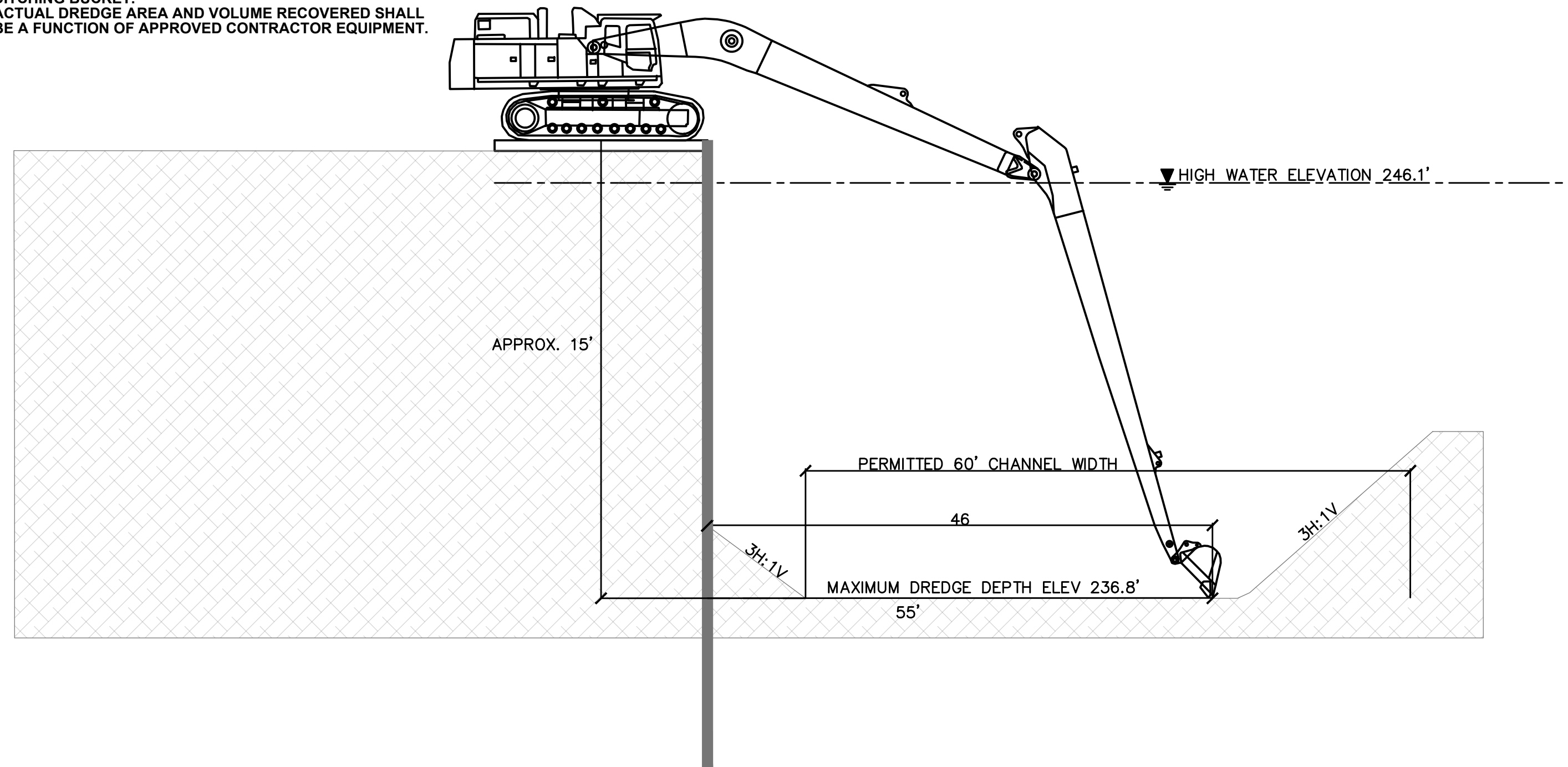
CROSS SECTIONS

DRAWING NUMBER

G-5

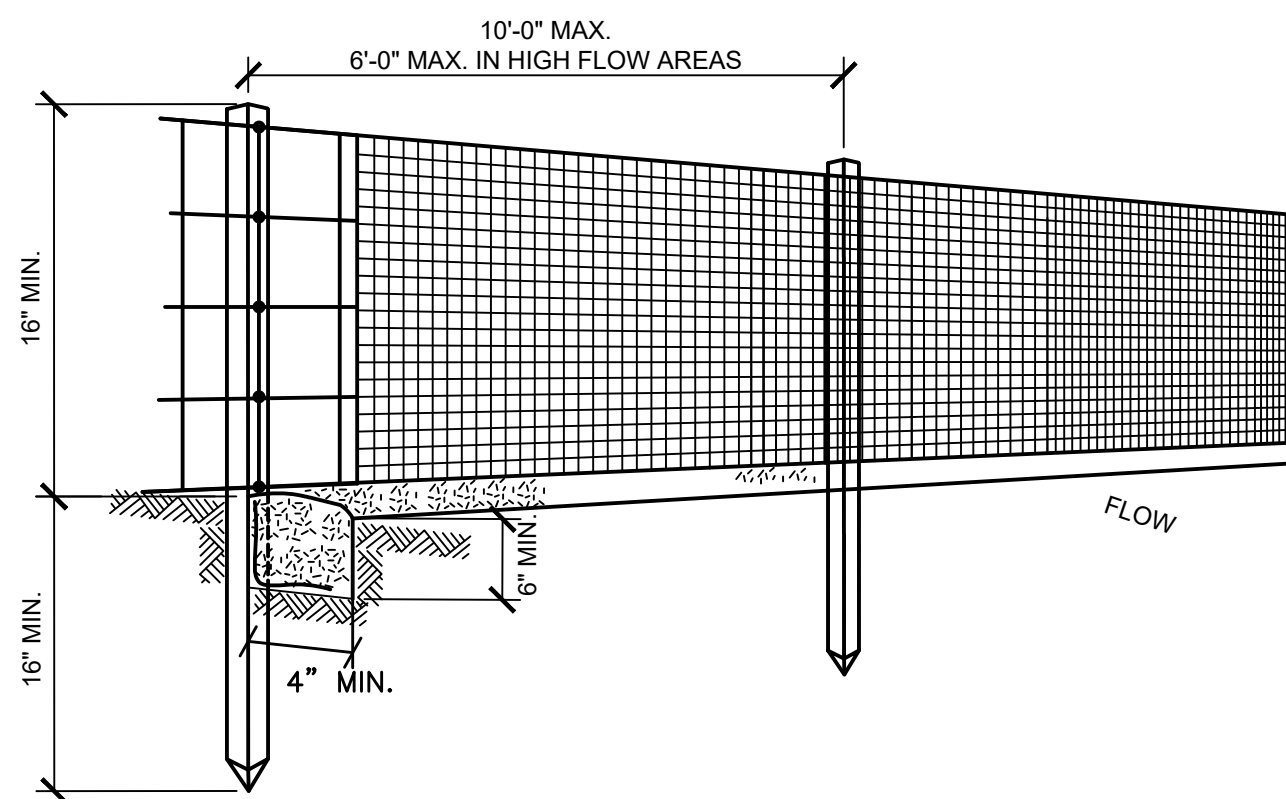
SHEET NUMBER
6 OF 7

- NOTES:**
1. MAX DREDGE WIDTH ESTIMATED BASED ON ASSUMED REACH OF LONG REACH (60') EXCAVATOR FITTED WITH DITCHING BUCKET.
 2. ACTUAL DREDGE AREA AND VOLUME RECOVERED SHALL BE A FUNCTION OF APPROVED CONTRACTOR EQUIPMENT.



TYPICAL DREDGE CROSS SECTION

NOT TO SCALE

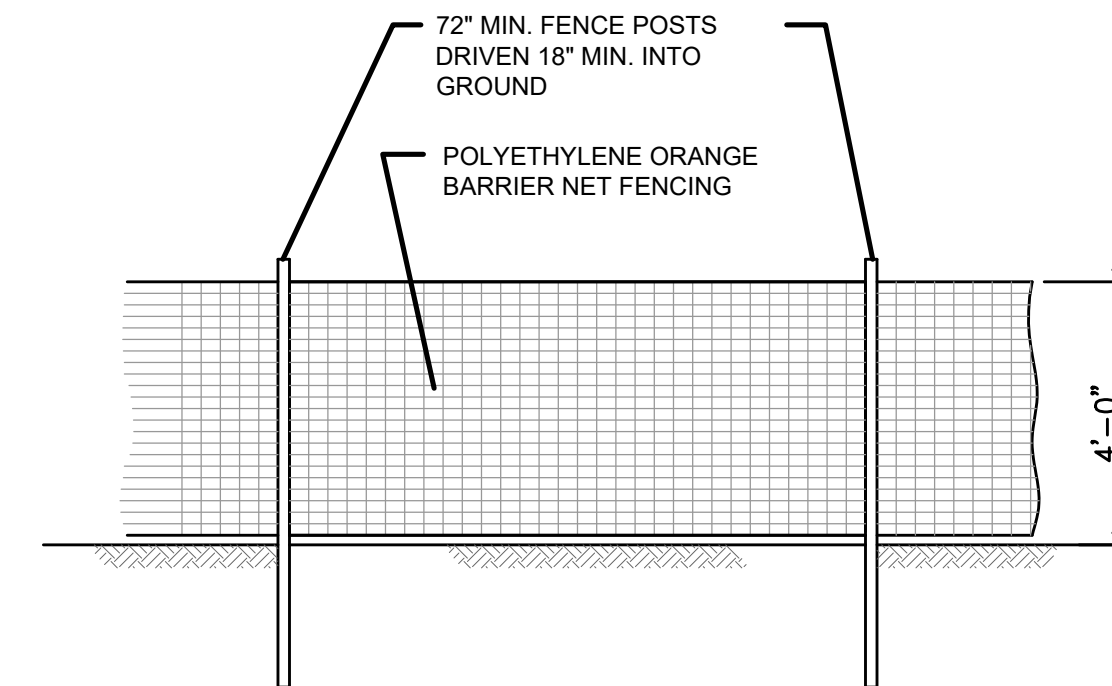


DETAIL NOTES:

1. WOVEN WIRE FENCE TO BE FASTENED SECURELY TO POSTS WITH WIRE TIES OR STAPLES.
 2. SEDIMENT CONTROL FABRIC TO BE FASTENED SECURELY TO WOVEN WIRE FENCE WITH TIES SPACED EVERY 24" AT TOP AND MID SECTION. EMBED SEDIMENT CONTROL FABRIC MIN. 6" INTO GROUND.
 3. WHEN TWO SECTIONS OF SEDIMENT CONTROL FABRIC ADJOIN EACH OTHER THEY SHALL BE OVER-LAPPED BY MIN. SIX INCHES AND FOLDED.
 4. MAINTENANCE SHALL BE PERFORMED AS NEEDED AND MATERIAL REMOVED WHEN "BULGES" DEVELOP IN THE SILT FENCE.
 5. FENCE TO BE ALIGNED ALONG CONTOUR AS CLOSELY AS POSSIBLE.
- POSTS : STEEL (EITHER "T" OR "U" TYPE) OR 2" HARDWOOD ALL MIN. 36" LENGTH.
- FENCE : WOVEN WIRE; MIN. 14.5 GAUGE 6" MAX. MESH OPENING
- SEDIMENT CONTROL FABRIC : MINIMUM TENSILE STRENGTH OF 120 LBS/100 LBS (MD/CD) (ASTM D4632)
- PREFABRICATED UNIT : MIRAFI ENVIROFENCE, OR APPROVED EQUAL

SILT FENCE DETAIL

NOT TO SCALE



NOTES:

1. SECURE FENCING SECURELY TO POSTS.
2. MIN. 2" OVERLAP OF FENCE SPLICES (SECURE AS REQUIRED).
3. REMOVE UPON COMPLETION OF WORK.
4. INSTALL CONSTRUCTION FENCE ALONG PERIMETER OF WORK ZONE.
5. EQUIP FENCE WITH "CONSTRUCTION AREA AUTHORIZED PERSONNEL ONLY" AT A MINIMUM OF EVERY 20 FEET.

TEMPORARY CONSTRUCTION FENCE DETAIL

NOT TO SCALE

WARNING:
THE ALTERATION OF THIS MATERIAL IN ANY WAY, UNLESS DONE UNDER THE DIRECTION OF A COMPARABLE PROFESSIONAL, I.E. ARCHITECT FOR AN ARCHITECT, ENGINEER FOR AN ENGINEER OR LANDSCAPE ARCHITECT FOR A LANDSCAPE ARCHITECT, IS A VIOLATION OF THE NEW YORK STATE EDUCATION LAW AND/OR REGULATIONS AND IS A CLASS 'A' MISDEMEANOR.



O'BRIEN & GERE ENGINEERS, INC
A RAMBOLL COMPANY



**2019
PORT BAY
DREDGING
IMPROVEMENTS**

REVISIONS

REV	DATE	DESCRIPTION
0	12/24/19	ISSUED FOR CONSTRUCTION

LINE IS 2 INCHES
AT FULL SIZE

DESIGNED: R. NEWBY

DRAWN: A. WETHERBEE

CHECKED: T. STANIEC

CHECKED: S. ANAGNOST

APPROVED: T. MADDEN

FILENAME
73070.300 - G-6 MISC DETAILS.DWG

PROJECT NUMBER
73070

CLIENT PROJECT NUMBER
46224

**MISCELLANEOUS
DETAILS**

DRAWING NUMBER

G-6

SHEET NUMBER
7 OF 7